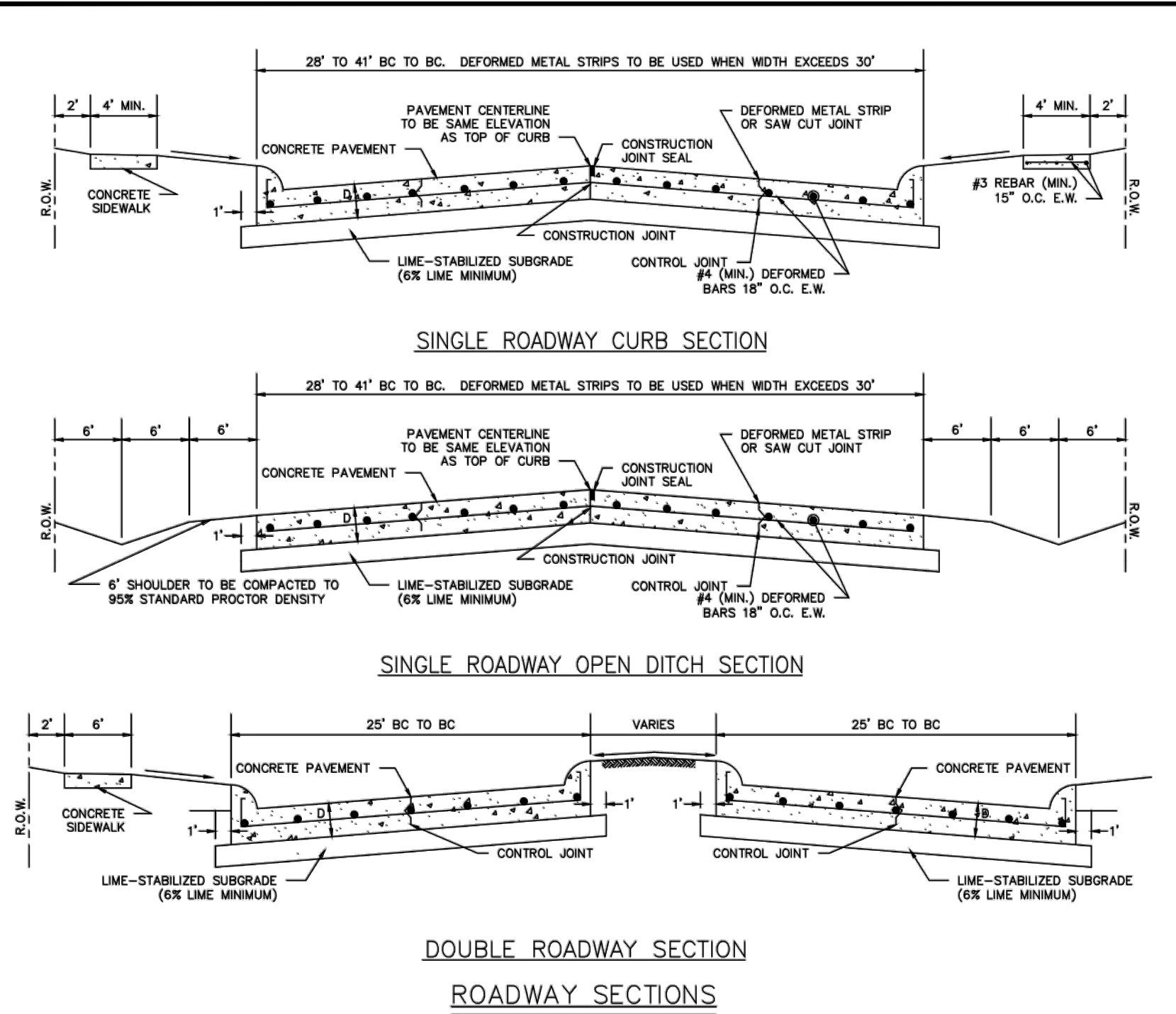
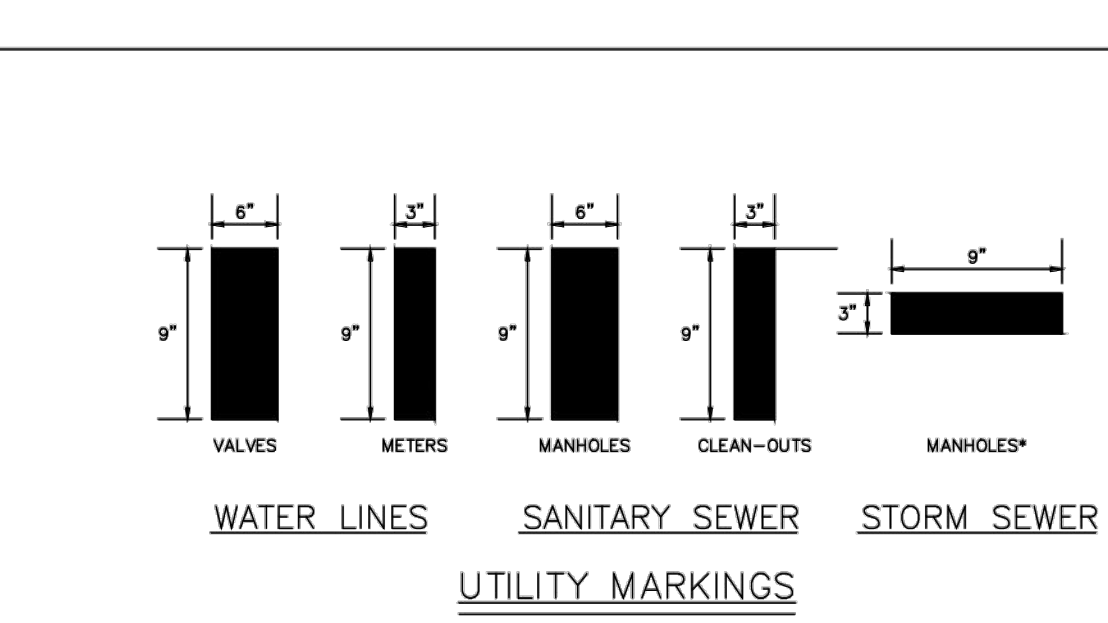
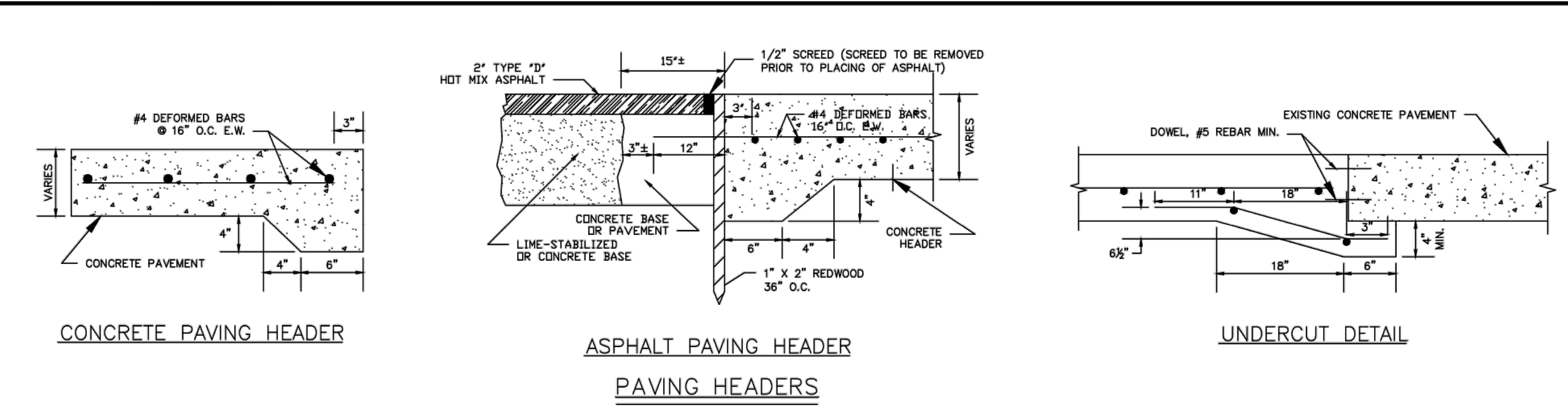


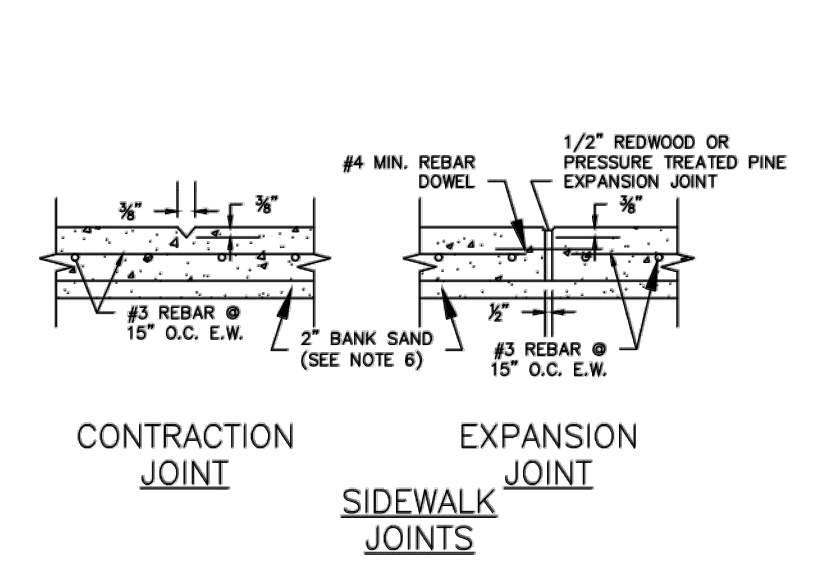
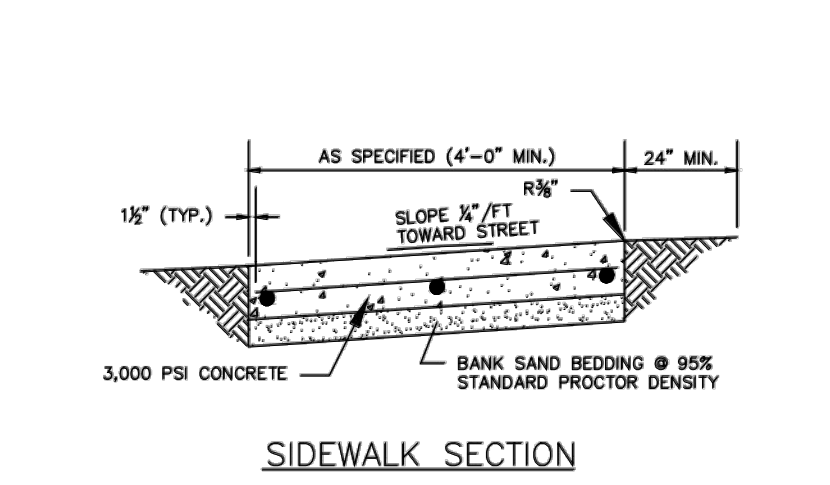
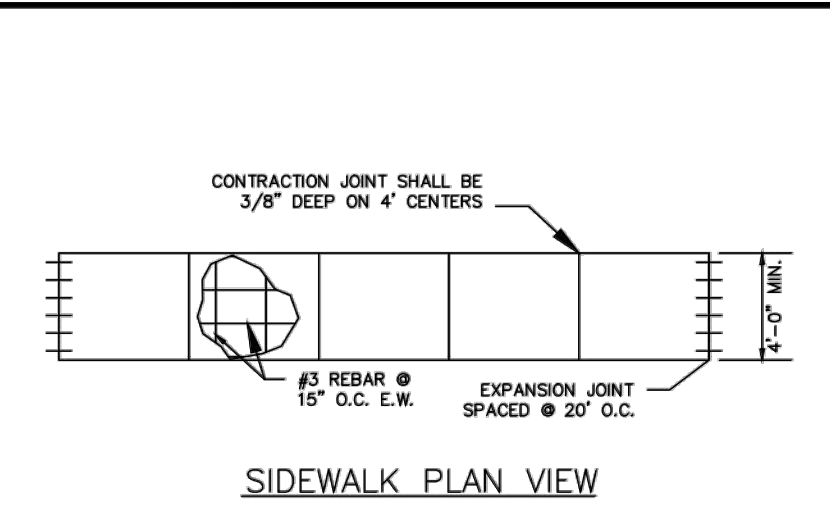
REVISIONS AND ISSUANCE		
NO.	DATE	DESCRIPTION



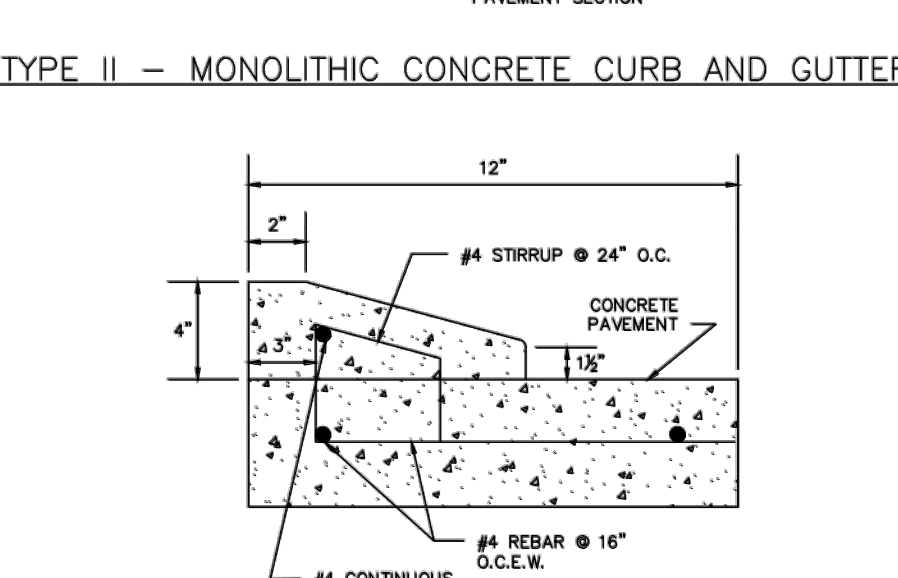
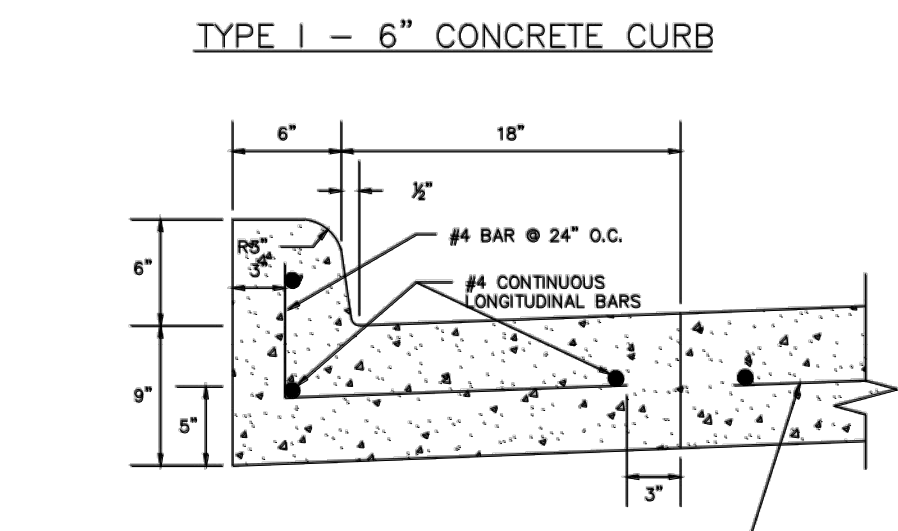
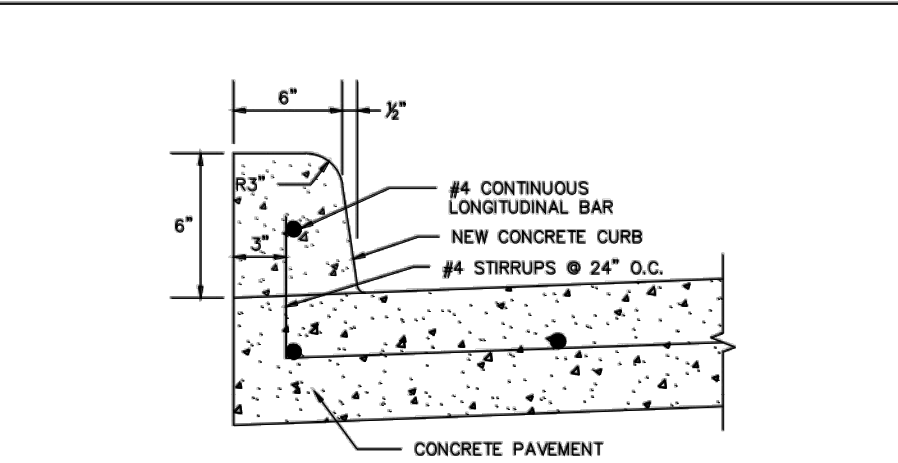
- NOTES:
- FOR ALL SIX (6) AND SEVEN (7) INCH PAVING USE #4 REBAR ON EIGHTEEN (18) INCH CENTERS EACH WAY.
  - ALL REINFORCING BAR SHALL CONFORM TO ASTM A113-S47 AND A603-S31.
  - THE LOCATION OF CONSTRUCTION JOINTS, CONTROL JOINTS AND DEFORMED STRIPS MAY BE VARIED, WITH THE APPROVAL OF THE CITY OF MANVEL, TO SUIT THE PROPOSED CONSTRUCTION METHOD OF THE CONTRACTOR. THE MAXIMUM LENGTH BETWEEN LONGITUDINAL JOINTS SHALL NOT EXCEED 18'-0".
  - ALL PAVEMENT CROSS-SECTION SHALL EITHER BE HYPERBOLIC OR TANGENT CROWN. MINIMUM SLOPE SHALL BE ONE-QUARTER (1/4) INCH PER FOOT.
  - D = SIX (6) INCH OR SEVEN (7) INCH. SEE PAVING CONSTRUCTION ITEMS ON THIS SHEET.
  - MINIMUM SLOPE FOR ANY MEDIAN SHALL BE ONE-HALF (1/2) INCH PER FOOT.



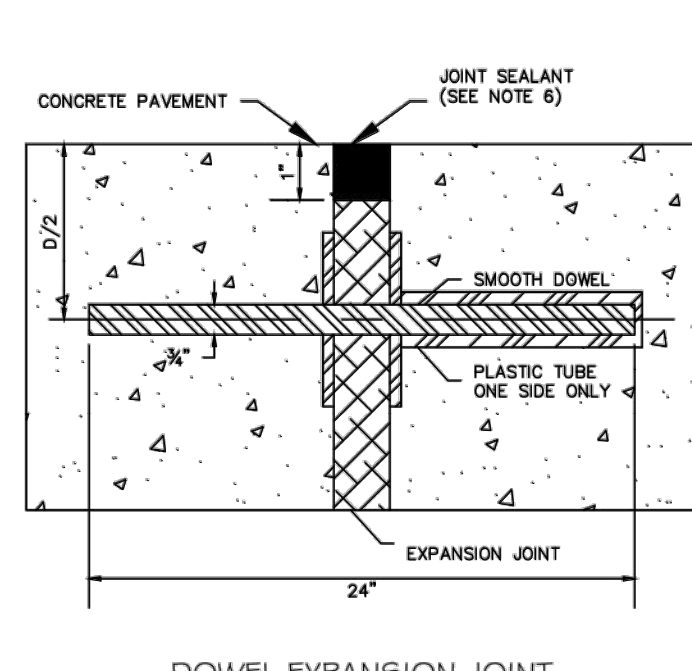
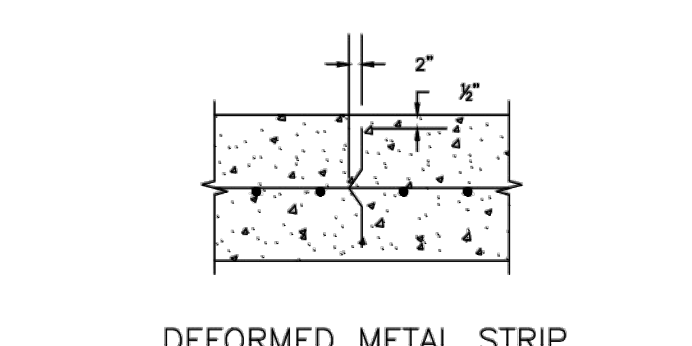
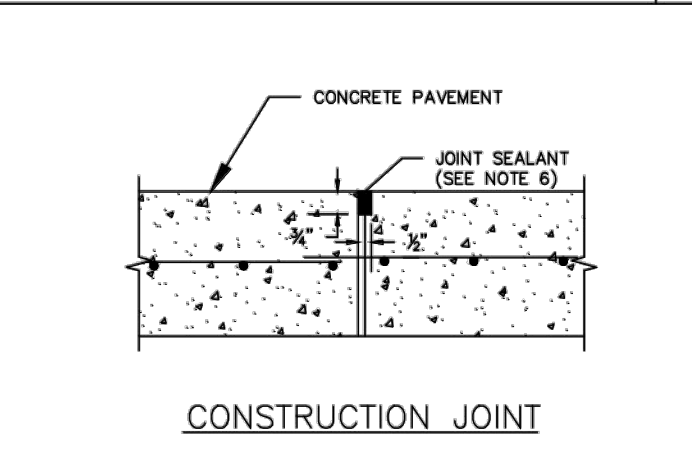
- NOTES:
- SAW CUT CURB OR EDGE AND APPLY APPROPRIATE MARKING AS LISTED BELOW.
  - WATER VALVE MARKING SHALL BE 6" X 9" IN SIZE MARKED IN BLUE PAINT ON CURB OR EDGE OF STREET.
  - WATER METER MARKING SHALL BE 3" X 9" IN SIZE MARKED IN BLUE PAINT ON CURB OR EDGE OF STREET.
  - SANITARY SEWER MANHOLE MARKING SHALL BE 6" X 9" IN SIZE MARKED IN GREEN PAINT ON CURB OR EDGE OF STREET.
  - SANITARY SEWER CLEAN-OUT MARKING SHALL BE 3" X 9" IN SIZE AND MARKED IN GREEN PAINT ON CURB OR EDGE OF STREET.
  - STORM SEWER MANHOLE MARKING SHALL BE 3" X 9" IN SIZE AND MARKED IN BLACK PAINT. ONLY STORM SEWER MANHOLES SHALL BE MARKED NOT, INLETS.



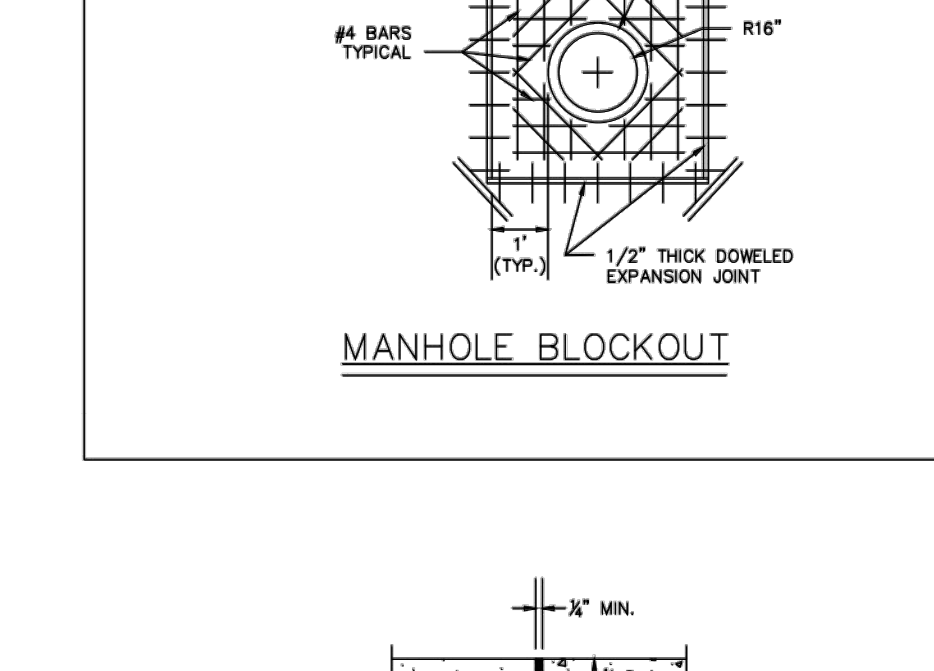
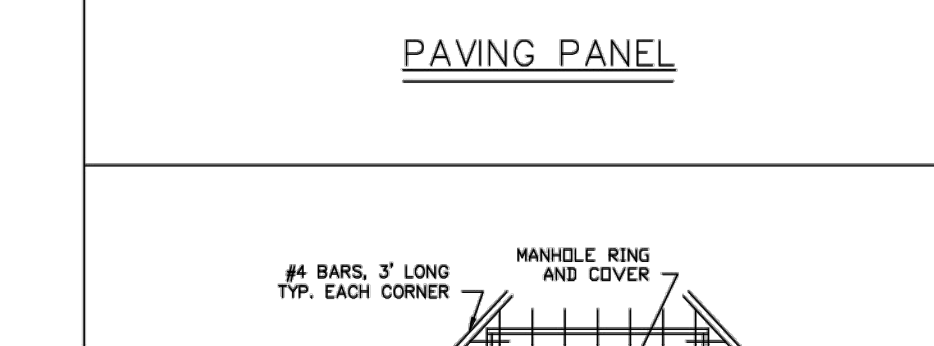
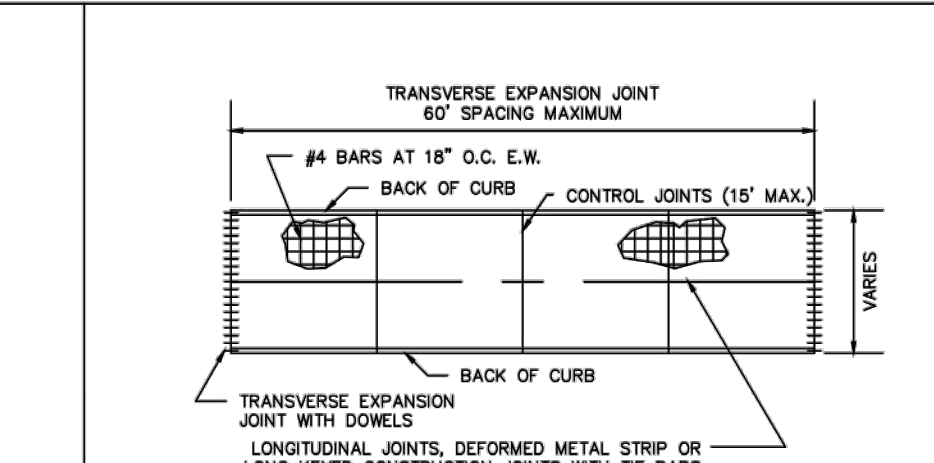
- NOTES:
- ALL EDGES SHALL BE ROUNDED WITH 3/8" RADIUS.
  - CONTRACTION JOINTS SHALL BE PLACED AT 4'-0" INTERVALS.
  - CONTRACTION JOINTS SHALL BE 3/8" DEEP AND HAVE TROWEL EDGE.
  - 1/2" EXPANSION JOINT MATERIAL REQUIRED WHERE SIDEWALKS ADJUT BUILDINGS, CURBS, DRIVEWAYS, OR EXISTING SIDEWALKS.
  - EXPANSION JOINTS SHALL BE PLACED AT 20'-0" INTERVALS.
  - 2" INCH CLEAN BANK SAND SHALL BE TAMPED TO 98% STANDARD PROCTOR.
  - WHERE NEW SIDEWALK IS PLACED AGAINST EXISTING SIDEWALK, SAW CUT EXISTING SIDEWALK TO AN EVEN STRAIGHT LINE PRIOR TO INSTALLATION OF THE NEW SIDEWALK. INSTALL #4 REBAR DOWEL INTO EXISTING SIDEWALK.



- NOTES:
- WHEN CONCRETE CURB IS TO BE PLACED ON EXISTING CONCRETE BASE #4 DEFORMED BARS, 7' LONG, 24" O.C. DOWELED 4" MIN. AND SET IN 2" MIN.
  - REDWOOD EXPANSION JOINTS SHALL BE INSTALLED AT ALL PAVEMENT EXPANSION JOINTS.
  - MORTAR FINISH IS NOT REQUIRED WHEN CURB IS PLACED BY A MACHINE, BUT CURB SHALL STILL HAVE THE SAME OUTSIDE DIMENSIONS.



- NOTES:
- CANTILEVER TYPE, CAST MALLEABLE IRON LOAD TRANSMISSION UNIT-STABILUG MODEL D-27 OR EQUAL, ON 22" O.C. ARE ACCEPTABLE ALTERNATIVES.
  - EXPANSION JOINT TO BE PLACED AT THE END OF EACH CURB RADIUS AND SPACED A MINIMUM OF 60' APART. MATCH TO EXPANSION JOINTS IN PAVING PANELS.
  - STAPLES FOR TRANSVERSE JOINTS SHALL NOT BE PLACED CLOSER THAN 6" TO A LONGITUDINAL JOINT. THE TOP OF STAKE SHALL NOT BE LESS THAN 1/2" BELOW THE FINISH SURFACE.
  - LOCATION OF CONSTRUCTION JOINTS AND DEFORMED STRIPS MAY BE VARIED WITH THE APPROVAL OF THE CITY, TO SUIT PROPOSED CONSTRUCTION OF THE CONTRACTOR.
  - CONTRACTION JOINTS SHALL BE SPACED AT A MAXIMUM OF 19'-0".
  - JOINTS AND SEALANTS SHALL ADHERE TO CITY OF MANVEL TECHNICAL SPECIFICATION 02840 - CONCRETE PAVEMENT JOINTS.

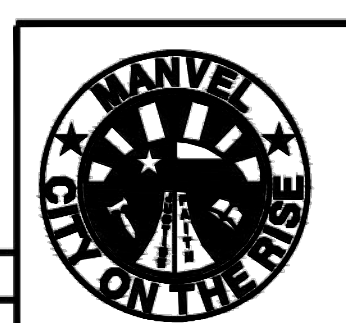


- NOTES:
- D = DEPTH OF CONCRETE.
  - SAW-CUT CONTRACTION JOINT TO DIMENSIONS SHOWN.

- NOTES:
- RAMP SHALL MEET ADA (AMERICANS WITH DISABILITIES ACT) AND TABA (TEXAS ARCHITECTURAL BARRIERS ACT).
  - CONTRACTOR SHALL MAKE ADJUSTMENTS TO RAMP TO FIT LOCAL CONDITION.
  - ALL CUTOITS MUST HAVE BARRIAGES INSTALLED UNTIL WHEELCHAIR RAMPS ARE COMPLETED.
  - MINIMUM CLEARANCE (WIDTH) OF RAMP SHALL BE SIXTY (60) INCHES.
  - FINISH SURFACES OF RAMP AND SIDE SLOPE SHALL BE TREATED WITH COLOR SO THAT THE SURFACES ARE IN CONTRAST WITH SURFACE COLOR OF ADJACENT SIDEWALKS AND STREETS.
  - CONTRAST COLOR OF WHEELCHAIR RAMPS AND SIDE SLOPES SHALL BE BLACK DYE.
  - CROSS SLOPE NOT TO EXCEED 2% ON ANY PORTION OF RAMP, LANDING OR TRANSITION TO STREET.

**SIDEWALKS AND WHEELCHAIR RAMPS**

- PAVING:
- PAVEMENT SUBGRADE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF MANVEL DESIGN CRITERIA MANUAL, LATEST REVISIONS.
  - CONTRACTOR SHALL NOTIFY THE CITY, TWENTY-FOUR (24) HOURS PRIOR TO ALL LIMING AND PAVING OPERATIONS.
  - ALL RETURNS SHALL HAVE A TWENTY-FIVE (25) FOOT RADIUS AT BACK OF CURB UNLESS OTHERWISE NOTED.
  - GUIDELINES SET FORTH IN THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" SHALL BE OBSERVED.
  - ALL FILL IN EXISTING OR PROPOSED RIGHTS-OF-WAY, INCLUDING BACKDRESSING BEHIND THE CURBS, SHALL BE PLACED IN MAXIMUM LOOSE LIFTS OF EIGHT (8) INCHES OR LESS AND COMPACTED TO NINETY-FIVE PERCENT (95%) STANDARD PROCTOR DENSITY WITH AN OPTIMUM MOISTURE CONTENT OF ± 3%.
  - MINIMUM PAVEMENT REINFORCEMENT REQUIREMENT SHALL BE GRADE 60, NO. 4 REBAR, SPACED AT EIGHTEEN (18) INCH ON CENTERS EACH WAY.
  - ALL PAVEMENT TO BE A MINIMUM OF SIX (6) INCHES THICK REINFORCED CONCRETE UNLESS OTHERWISE NOTED.
  - PAVING EXPANSION JOINTS SHALL BE PLACED AT A MAXIMUM OF SIXTY (60) FEET.
  - ALL CONCRETE USED FOR PAVEMENT SHALL BE CLASS "A" CONCRETE AND A MINIMUM 4,000 PSI COMPRESSIVE STRENGTH AT TWENTY-EIGHT (28) DAYS.
  - CLASS "A" HYDRATED LIME SHALL BE APPLIED FOR SUBGRADE STABILIZATION AT A MINIMUM OF 6%.
  - CONTRACTOR SHALL INSTALL STREET SIGNS AND STOP SIGNS PER CITY OF MANVEL DESIGN CRITERIA MANUAL.
  - ALIGNMENTS, CENTERLINE CURVE DATA, AND STATIONING FOR ALL CONSTRUCTION SHALL BE DETERMINED FROM SUBDIVISION PLAT.
  - FOR ALL CONCRETE TO BE REMOVED, A TWO (2) INCH DEEP SAW CUT SHALL BE PROVIDED PRIOR TO REMOVAL.
  - REPRESENTATIVES FROM THE CITY, THE OWNER AND THE TESTING LABORATORY SHALL BE PRESENT FOR ALL DENSITY TESTS, LIME OPERATIONS AND PLACEMENT OF CONCRETE PAVING. NO INSPECTABLE WORK CAN BE CONDUCTED ON SATURDAYS.
  - UNDER NO CIRCUMSTANCES SHALL WATER BE ADDED TO A CONCRETE LOAD AFTER SLUMP TEST AND/OR CONCRETE CYLINDERS HAVE BEEN TAKEN.
  - BLUE REFLECTORIZED PAVEMENT MARKERS SHALL BE PLACED AT FIRE HYDRANT LOCATIONS AND OFFSET SIX (6) INCHES FROM THE CENTERLINE OF THE ROADWAY. REFLECTORS SHALL FACE FLOW OF TRAFFIC.
  - FOR PAVEMENT WIDTHS LESS THAN OR EQUAL TO TWENTY-EIGHT (28) FEET BB OF CURB:
    - MINIMUM STABILIZED SUBGRADE THICKNESS SHALL BE SIX (6) INCHES.
    - MINIMUM CONCRETE SLAB THICKNESS SHALL BE SIX (6) INCHES.
  - FOR PAVEMENT WIDTHS GREATER THAN TWENTY-EIGHT (28) FEET BB OF CURB AND ALL MAJOR ARTERIAL THOROUGHFARES:
    - MINIMUM STABILIZED SUBGRADE THICKNESS SHALL BE EIGHT (8) INCHES.
    - MINIMUM CONCRETE SLAB THICKNESS SHALL BE SEVEN (7) INCHES.



**PAVING STANDARD DETAILS**

DATE APPROVED: JUNE 2018  
SCALE: NTS  
REVISED DATE: JUNE 2018

PROJECT NUMBER:      DATE SUBMITTED:      SHEET:      XX OF XX

**IVY KIDS EARLY LEARNING CENTER**  
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(713) 981-0018  
[E] dzhuang@missioneng.com  
TPEF Registration No. F-11771



04/16/2019

**CONSTRUCTION DETAILS**

DRAWN BY: KN      CHECKED BY: EL  
DATE: 04/16/2019      JOB NO.: 1806251

DRAWING NO. **C8**